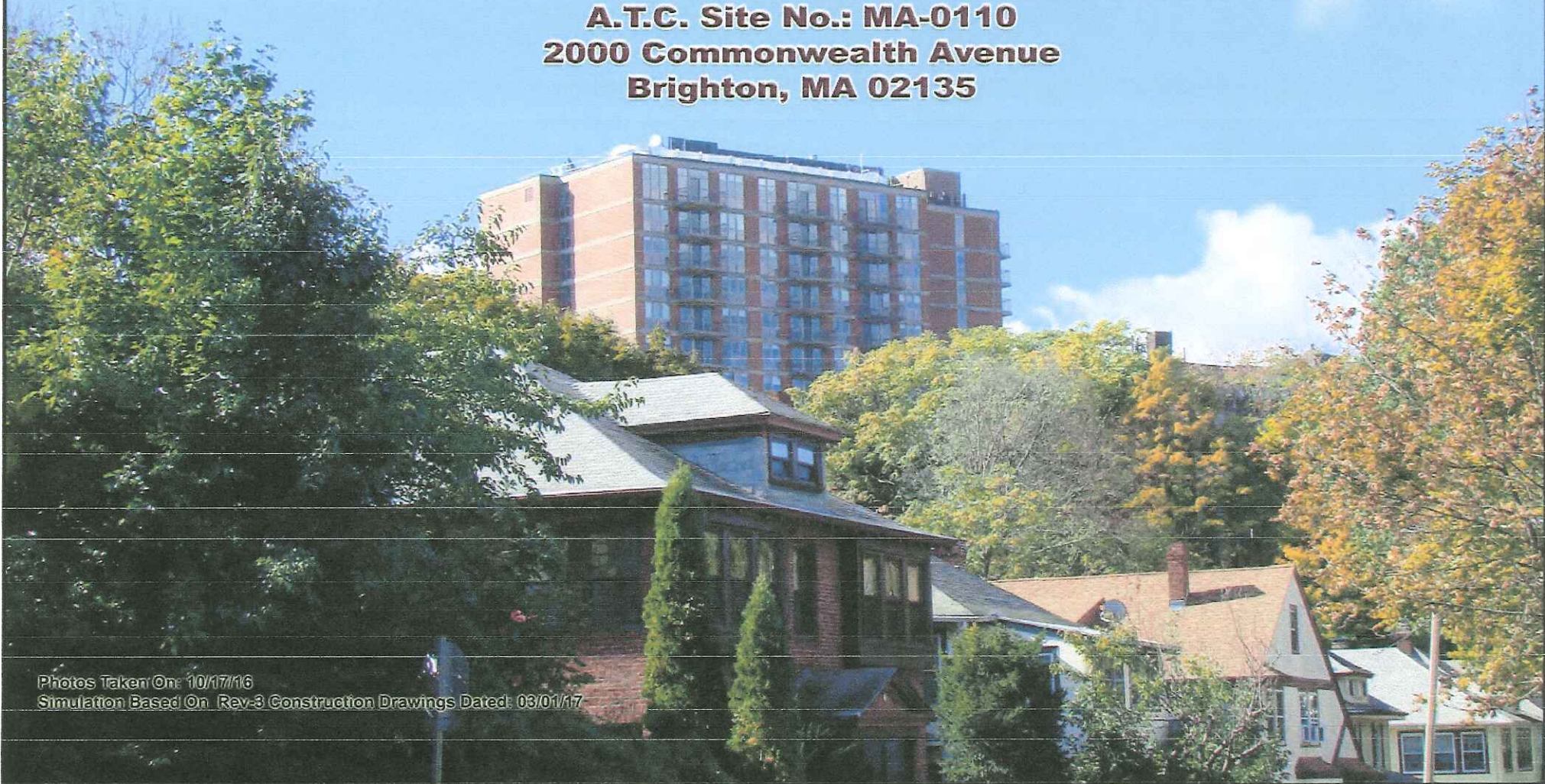


**Prepared For:
AT&T Mobility
Site Name:
2000 COMMONWEALTH AVENUE
BWE
Site No.: MA2016
A.T.C. Site No.: MA-0110
2000 Commonwealth Avenue
Brighton, MA 02135**



Photos Taken On: 10/17/16
Simulation Based On: Rev-3 Construction Drawings Dated: 03/01/17

For visual reference only. Actual visibility
is dependent upon weather conditions,
season, sunlight, and viewer location.



2000 COMMONWEALTH AVENUE BWE
Site No.: MA2016
A.T.C. Site No.: MA-0110
DEWBERRY NO. 50083655
(Page 1 of 9)





PHOTO 1

PHOTO 2

PHOTO 3

PHOTO 4

SITE LOCATION

30



2000 COMMONWEALTH AVE.
SITE NO.: MA2016
2000 Commonwealth Ave.
Brighton, MA 02135
(Page 2 of 9)



Actual View



2000 COMMONWEALTH AVE.

Photo 1A
View Facing East
From Gerald Rd.
(Page 3 of 9)



Proposed View

Proposed RRU (Typ.-1)

Existing RRUs (To Be Painted To Match)

Proposed Gamma Sector Antenna
(Typ.-1) (To Replace Existing)

Proposed RRU (To Replace Existing) (Typ.-1)

Proposed Alpha Sector Antenna
(Typ.-1) (To Replace Existing)



2000 COMMONWEALTH AVE.

Photo 1B

View Facing East

From Gerald Rd.

(Page 4 of 9)



Actual View



2000 COMMONWEALTH AVE.

Photo 2A

View Facing South
From Kirkwood Road
(Page 5 of 9)



Proposed View

Proposed RRU Mounted Behind Existing RRU (Typ.-1)

Proposed Alpha Sector Antenna (Typ.-1) (To Replace Existing)

Proposed RRU (To Replace Existing) (Typ.-1)



Proposed RRUs (To Be Painted To Match)



2000 COMMONWEALTH AVE.

Photo 2B

View Facing South
From Kirkwood Road
(Page 6 of 9)



Actual View

Proposed Equipment Is
Not Visible From This Location



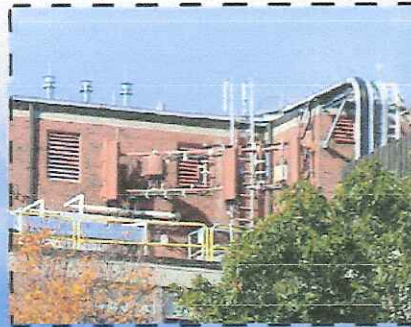
2000 COMMONWEALTH AVE.

Photo 3

View Facing West
From Commonwealth Ave.
(Page 7 of 9)



Actual View



2000 COMMONWEALTH AVE.

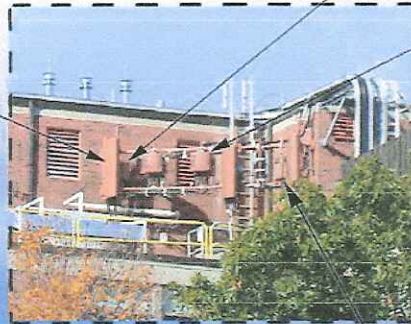
Photo 4A

View Facing Northwest
From Chestnut Hill Driveway
(Page 8 of 9)

 **Dewberry**[®]

Proposed View

Proposed Beta Sector Antenna
(Typ.-1) (To Replace Existing)



Proposed RRU (To Replace Existing)
Behind Existing Antenna (Typ.-1)

Proposed RRU (Typ.-1)

Existing RRUs
(To Be Painted To Match)



2000 COMMONWEALTH AVE.

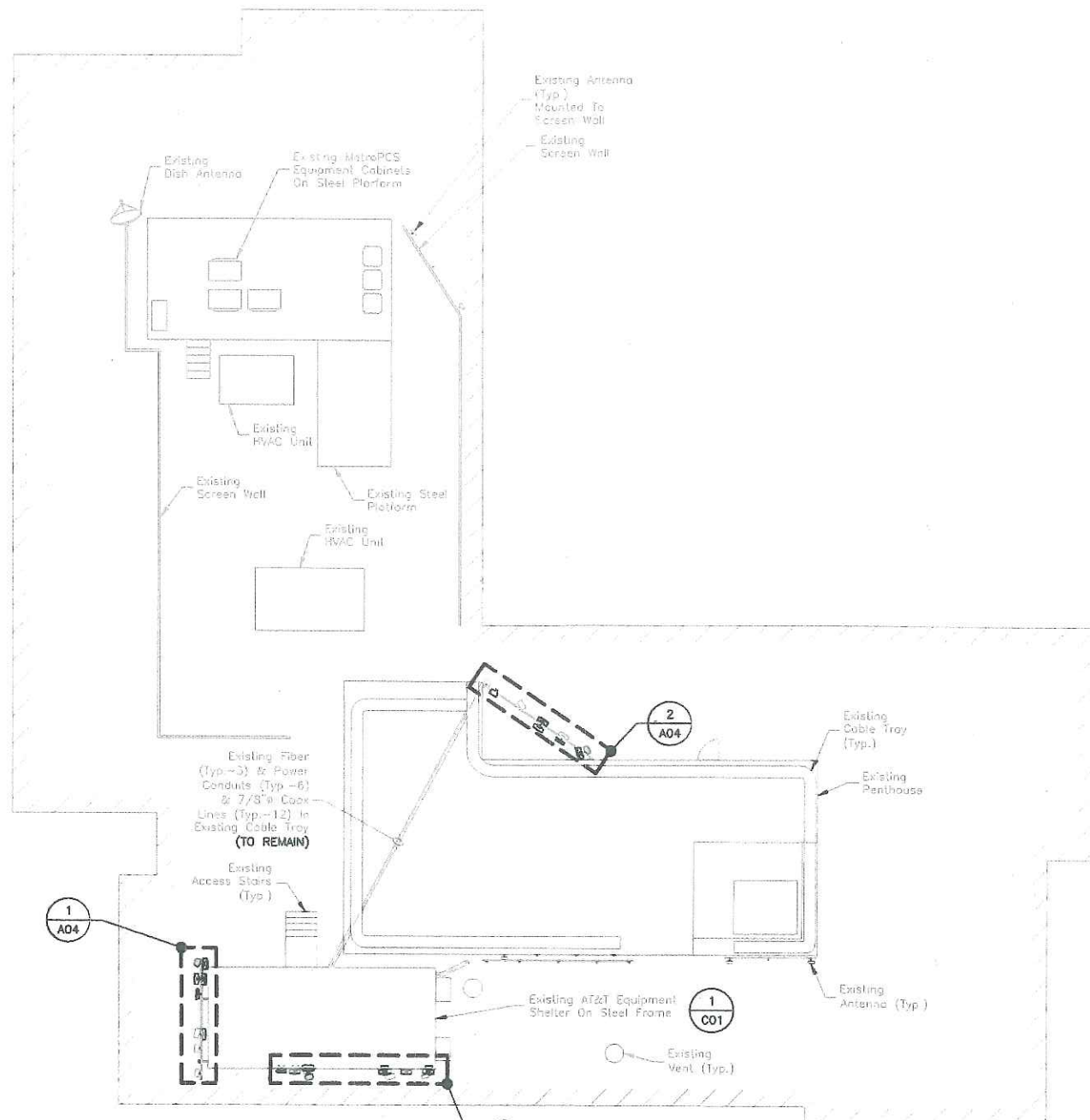
Photo 4B

View Facing Northwest
From Chestnut Hill Driveway
(Page 9 of 9)





COMMONWEALTH AVENUE



ROOF PLAN
 SCALE: 1"=20' FOR 11"x17"
 1"=10' FOR 22"x34"
 0' 10' 20'

- NOTES:**
1. NORTH SHOWN AS APPROXIMATE.
 2. SOME PROPOSED & EXISTING INFORMATION NOT SHOWN FOR CLARITY.
 3. ROOF PLAN BASED ON FIELD RECONNAISSANCE BY DEWBERRY ENGINEERS.
 4. REUSE EXISTING PIPE MOUNTS FOR PROPOSED ANTENNAS UNLESS NOTED OTHERWISE. INSPECT MOUNTS FOR DAMAGE OR DETERIORATION & REPLACE AS NECESSARY.
 5. ALL JUMPERS TO BE NEATLY BUNDLED BEHIND THE ANTENNAS & RRUS.
 6. INSTALL ALL PROPOSED EQUIPMENT PER MANUFACTURERS RECOMMENDATIONS & IN ACCORDANCE WITH STRUCTURAL ANALYSIS BY DEWBERRY ENGINEERS INC. DATED 10/10/16.
 7. ALL EXISTING AND PROPOSED RRUS & ANTENNAS SHALL BE PAINTED TO MATCH.

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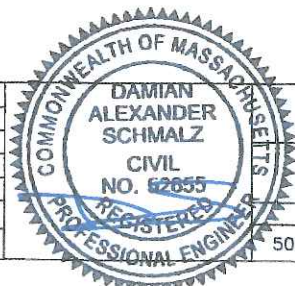
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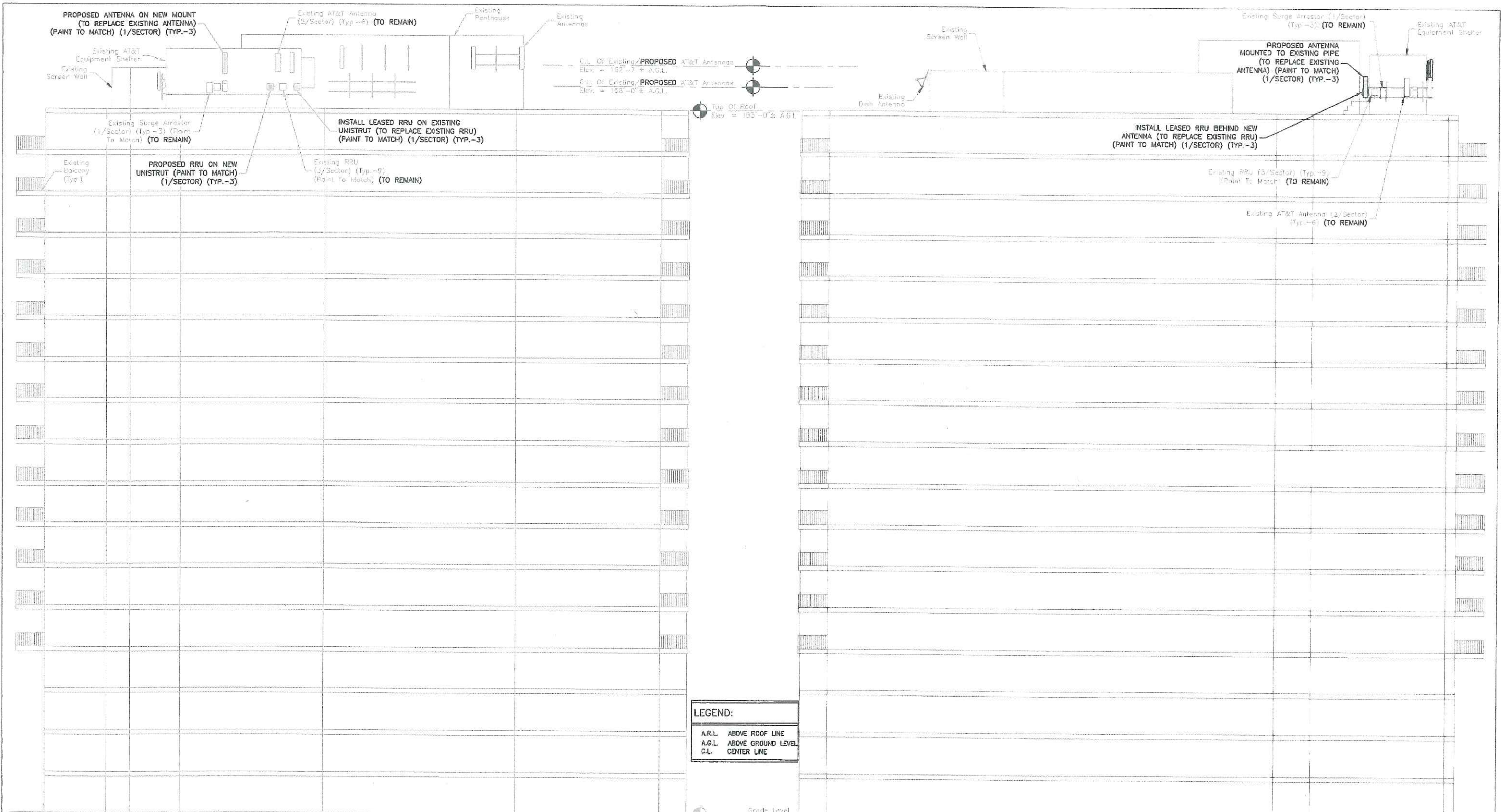
at&t Mobility
 550 COCHITUATE ROAD
 SUITES 13 & 14
 FRAMINGHAM, MA 01701

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3	03/01/17	ISSUED FOR CONSTRUCTION	MR	KB	DAS
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SCALE: AS SHOWN DESIGNED BY: KB DRAWN BY: MR



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ROOF PLAN	
DEWBERRY NO. 50019239/50083655	DRAWING NUMBER A01
	REV 3



LEGEND:

A.R.L.	ABOVE ROOF LINE
A.G.L.	ABOVE GROUND LEVEL
C.L.	CENTER LINE

EAST ELEVATION
 SCALE: 1"=20' FOR 11"x17"
 1"=10' FOR 22"x34"
 0' 10' 20'

NORTH ELEVATION
 SCALE: 1"=20' FOR 11"x17"
 1"=10' FOR 22"x34"
 0' 10' 20'

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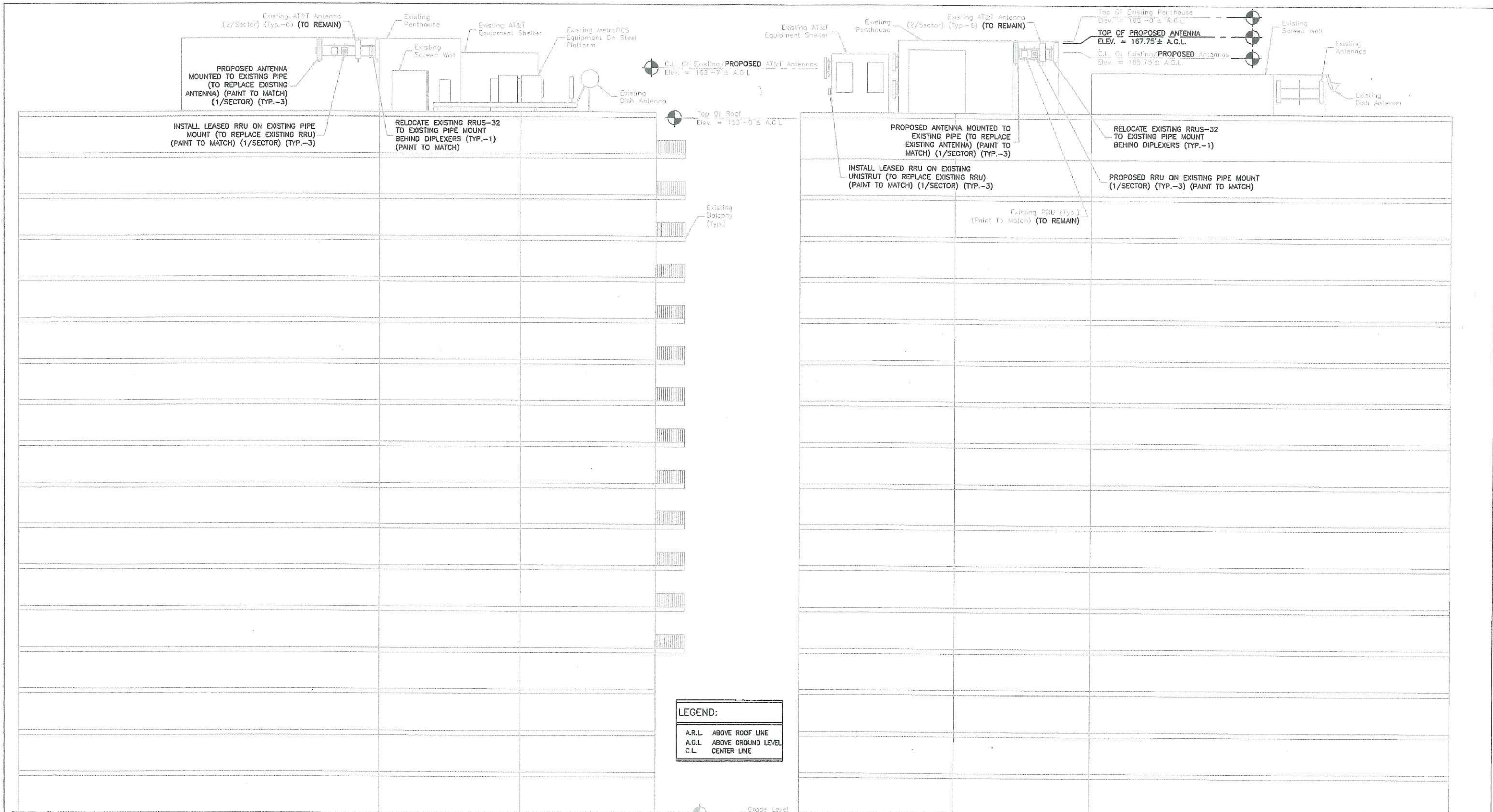
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 EAST & NORTH ELEVATIONS
 DEWBERRY NO. 50019239/50083655
 DRAWING NUMBER A02
 REV 3



LEGEND:

A.R.L.	ABOVE ROOF LINE
A.G.L.	ABOVE GROUND LEVEL
C.L.	CENTER LINE

WEST ELEVATION 1
 SCALE: 1"=20' FOR 11'x17"
 1"=10' FOR 22'x34"
 0' 10' 20'

SOUTH ELEVATION 2
 SCALE: 1"=20' FOR 11'x17"
 1"=10' FOR 22'x34"
 0' 10' 20'

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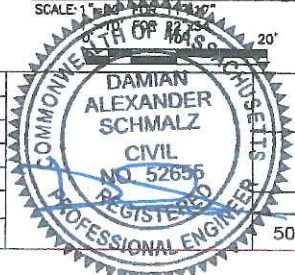
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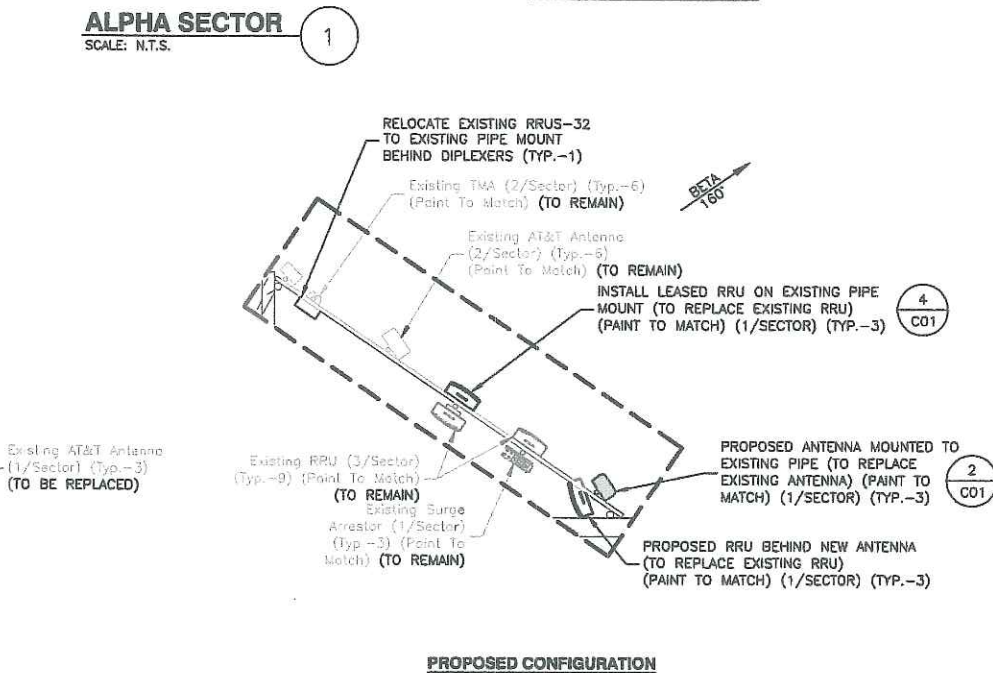
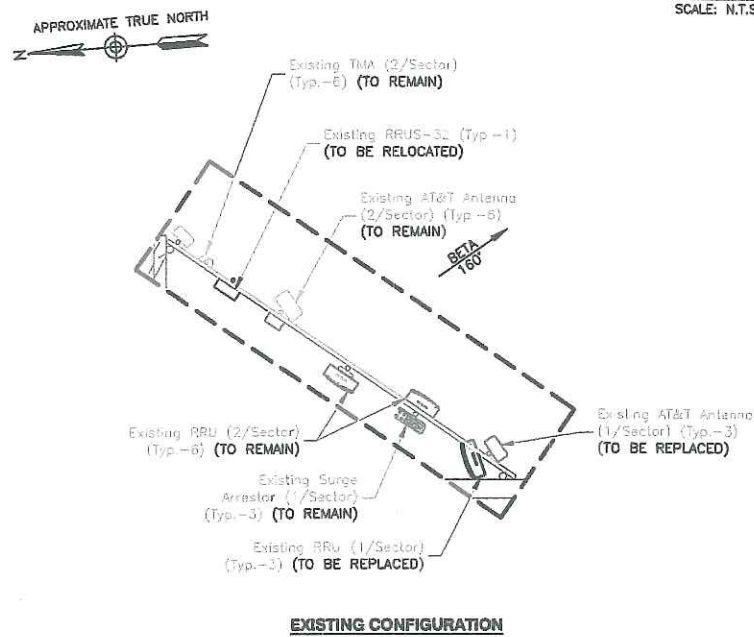
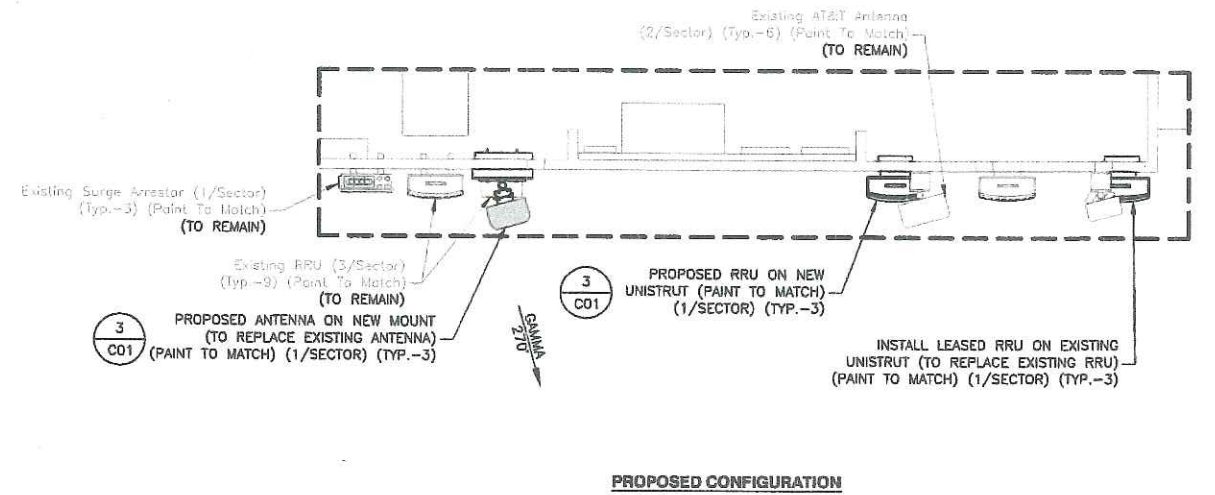
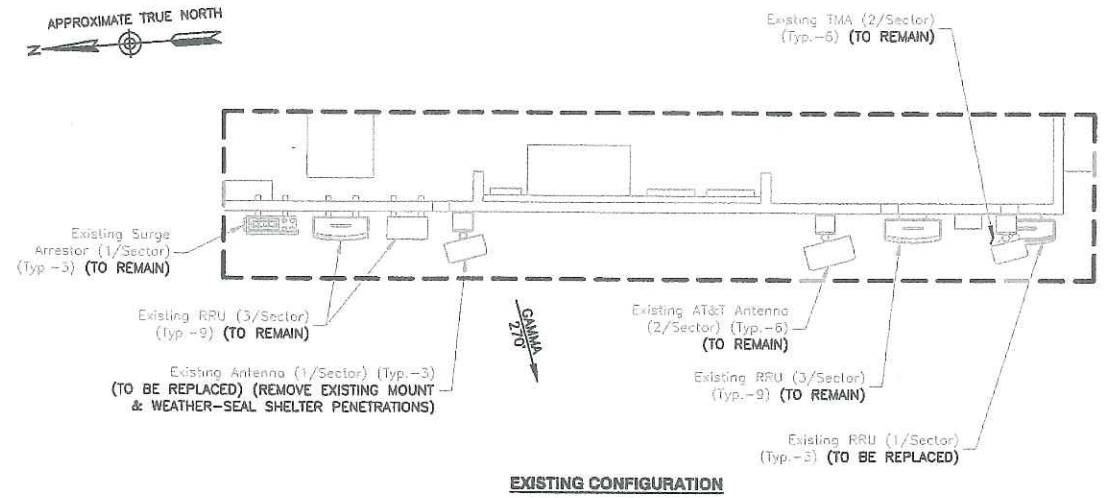
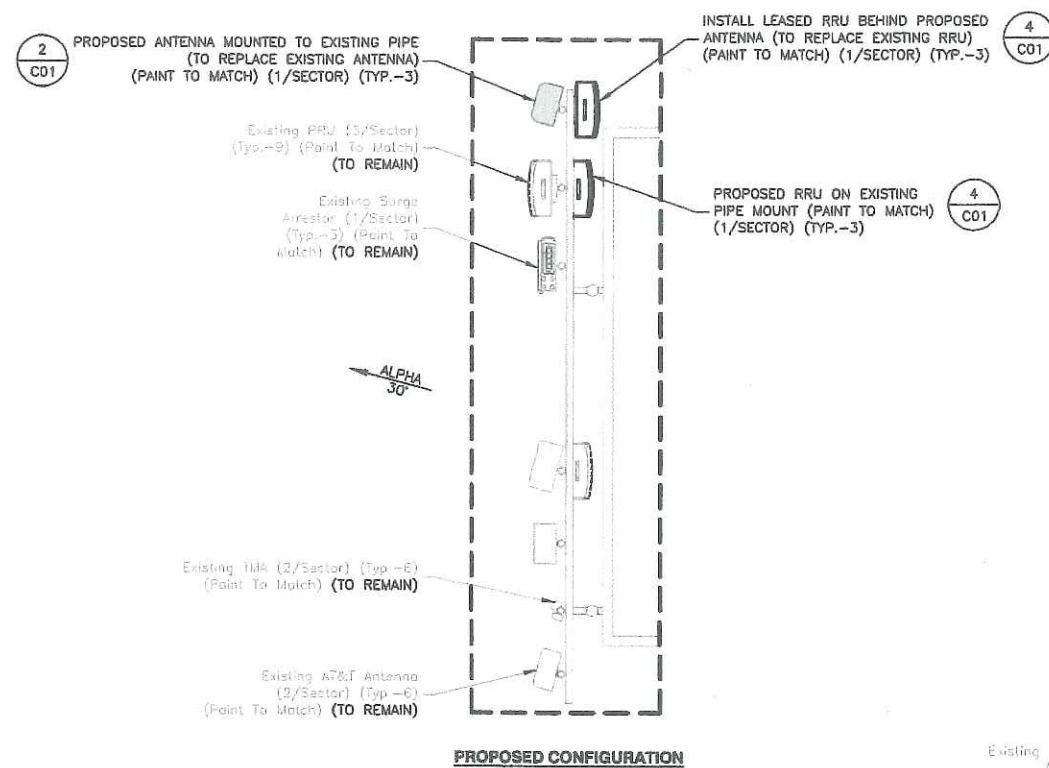
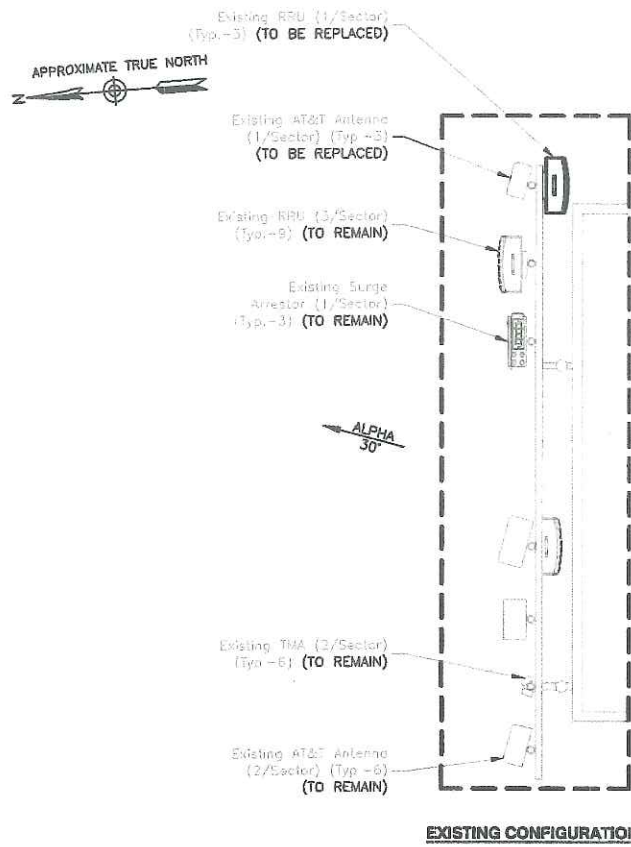
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0	08/18/16	ISSUED FOR REVIEW	MR	KB	DAS

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AT&T MOBILITY FRAMINGHAM, MA 01701	
WEST & SOUTH ELEVATIONS	
DEWBERRY NO.	DRAWING NUMBER
50019239/50083655	A03
	REV
	3



BETA SECTOR
SCALE: N.T.S.

GAMMA SECTOR
SCALE: N.T.S.

NOTES:

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 2. SOME PROPOSED & EXISTING INFORMATION NOT SHOWN FOR CLARITY.
 3. ROOF PLAN BASED ON FIELD RECONNAISSANCE BY DEWBERRY ENGINEERS.
 4. REUSE EXISTING PIPE MOUNTS FOR PROPOSED ANTENNAS UNLESS NOTED OTHERWISE. INSPECT MOUNTS FOR DAMAGE OR DETERIORATION & REPLACE AS NECESSARY.
 5. ALL JUMPERS TO BE NEATLY BUNDLED BEHIND THE ANTENNAS & RRUS.
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- EXISTING AND PROPOSED RRUS & ANTENNAS SHALL BE PAINTED TO

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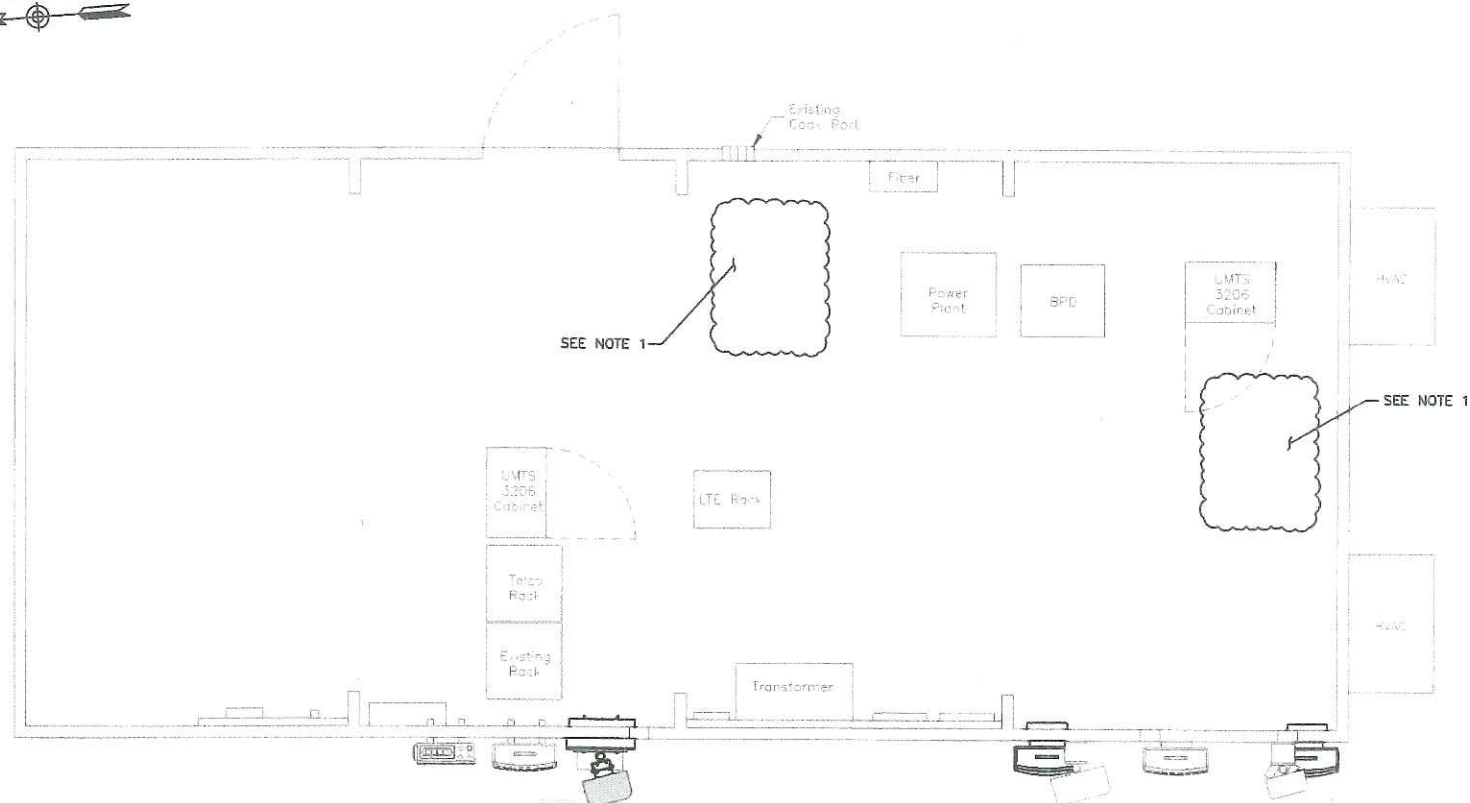


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ANTENNA ORIENTATION PLANS

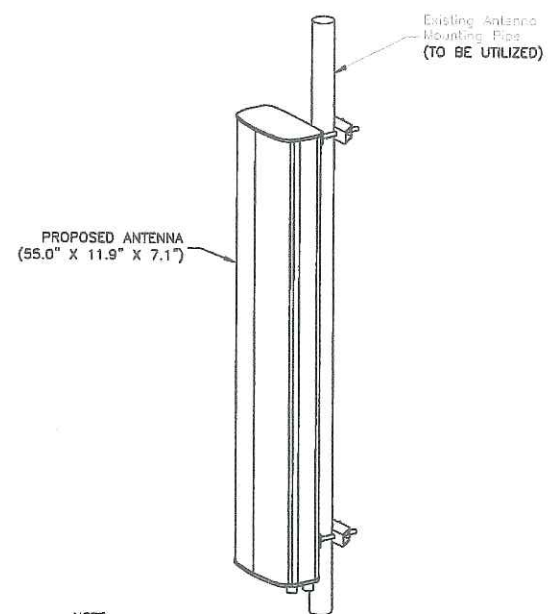
DEWBERRY NO.	DRAWING NUMBER	REV
50019239/50083655	A04	3

APPROXIMATE TRUE NORTH



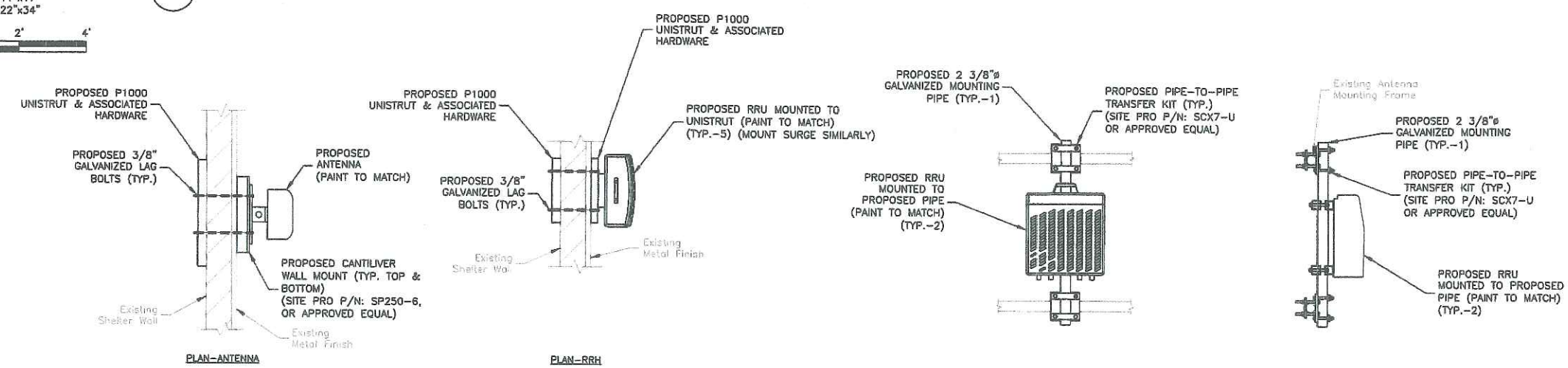
- NOTE:**
- EXISTING ROTTED SHELTER FLOOR LOCATIONS TO BE REPAIRED.
 - WEATHER-SEAL COAX PORTS TO PREVENT FURTHER WATER DAMAGE.
 - NO NEW RACKS OR CABINETS ARE TO BE INSTALLED INSIDE THE EQUIPMENT SHELTER AS PART OF THE CURRENT SCOPE OF WORK.

EQUIPMENT SHELTER PLAN 1
 SCALE: 1/4"=1' FOR 11"x17"
 1/2"=1' FOR 22"x34"
 0' 1' 2' 4'



- NOTE:**
- SEE RFDS FOR SPECIFIC ANTENNA MODEL.
 - REUSE EXISTING PIPE MOUNTS FOR PROPOSED ANTENNAS UNLESS NOTED OTHERWISE. INSPECT MOUNTS FOR DAMAGE OR DETERIORATION & REPLACE AS NECESSARY.

ISOMETRIC ANTENNA DETAIL 2
 SCALE: N.T.S.



SHELTER MOUNT DETAIL 3
 SCALE: 3/8"=1' FOR 11"x17"
 3/4"=1' FOR 22"x34"
 0' 1' 2' 3'

EQUIPMENT MOUNTING 4
 SCALE: 3/8"=1' FOR 11"x17"
 3/4"=1' FOR 22"x34"
 0' 2' 3'

- NOTES:**
- NORTH SHOWN AS APPROXIMATE.
 - SOME PROPOSED & EXISTING INFORMATION NOT SHOWN FOR CLARITY.
 - ROOF PLAN BASED ON FIELD RECONNAISSANCE BY DEWBERRY ENGINEERS.
 - REUSE EXISTING PIPE MOUNTS FOR PROPOSED ANTENNAS UNLESS NOTED OTHERWISE. INSPECT MOUNTS FOR DAMAGE OR DETERIORATION & REPLACE AS NECESSARY.
 - ALL JUMPERS TO BE NEATLY BUNDLED BEHIND THE ANTENNAS & RRUS.
 - INSTALL ALL PROPOSED EQUIPMENT PER MANUFACTURERS RECOMMENDATIONS & IN ACCORDANCE WITH STRUCTURAL ANALYSIS BY DEWBERRY ENGINEERS INC. DATED 10/10/16.
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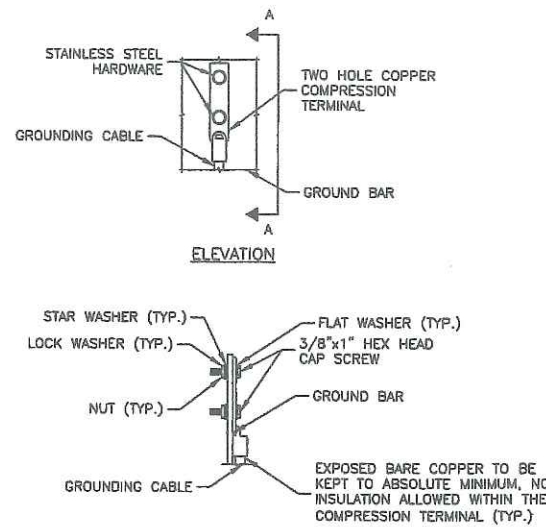
SCALE: AS SHOWN DESIGNED BY: KB DRAWN BY: MR



AT&T MOBILITY
 FRAMINGHAM, MA 01701
 EQUIPMENT SHELTER PLAN & CONSTRUCTION DETAILS
 DEWBERRY NO. 50019239/50083655 DRAWING NUMBER C01 REV 3

GROUNDING NOTES:

1. THE CONTRACTOR SHALL REVIEW AND INSPECT THE EXISTING FACILITY GROUNDING SYSTEM AND LIGHTNING PROTECTION SYSTEM (AS DESIGNED AND INSTALLED) FOR STRICT COMPLIANCE WITH THE NEC (AS ADOPTED BY THE AHJ), THE SITE-SPECIFIC (UL, LPI, OR NFPA) LIGHTNING PROTECTION CODE, AND GENERAL COMPLIANCE WITH TELCORDIA AND TIA GROUNDING STANDARDS. THE CONTRACTOR SHALL REPORT ANY VIOLATIONS OR ADVERSE FINDINGS TO THE CONTRACTOR FOR RESOLUTION.
2. ALL GROUND ELECTRODE SYSTEMS (INCLUDING TELECOMMUNICATION, RADIO, LIGHTNING PROTECTION, AND AC POWER GES'S) SHALL BE BONDED TOGETHER, AT OR BELOW GRADE, BY TWO OR MORE COPPER BONDING CONDUCTORS. ALL AVAILABLE GROUNDING ELECTRODES SHALL BE CONNECTED TOGETHER IN ACCORDANCE WITH THE NEC.
3. THE CONTRACTOR SHALL PERFORM IEEE FALL-OF-POTENTIAL RESISTANCE TO EARTH TESTING (PER IEEE 1100 AND 81) FOR GROUND ELECTRODE SYSTEMS. USE OF OTHER METHODS MUST BE PRE-APPROVED BY CONTRACTOR IN WRITING.
4. THE CONTRACTOR SHALL FURNISH AND INSTALL SUPPLEMENTAL GROUND ELECTRODES AS NEEDED TO ACHIEVE A TEST RESULT OF 5 OHMS OR LESS ON TOWER SITES AND 10 OHMS OR LESS ON ROOFTOP SITES WHEN ADDING ELECTRODES. CONTRACTOR SHALL MAINTAIN A MINIMUM DISTANCE BETWEEN THE ADDED ELECTRODE AND ANY OTHER EXISTING ELECTRODE EQUAL TO THE BURIED LENGTH OF THE ROD. IDEALLY, CONTRACTOR SHALL STRIVE TO KEEP THE SEPARATION DISTANCE EQUAL TO TWICE THE BURIED LENGTH OF THE RODS.
5. THE CONTRACTOR IS RESPONSIBLE FOR PROPERLY SEQUENCING GROUNDING AND UNDERGROUND CONDUIT INSTALLATION AS TO PREVENT ANY LOSS OF CONTINUITY IN THE GROUNDING SYSTEM OR DAMAGE TO THE CONDUIT.
6. METAL CONDUIT AND TRAY SHALL BE GROUNDING AND MADE ELECTRICALLY CONTINUOUS WITH LISTED BONDING FITTINGS OR BY BONDING ACROSS THE DISCONTINUITY WITH #6 AWG COPPER WIRE AND UL APPROVED GROUNDING TYPE CONDUIT CLAMPS.
7. METAL RACEWAY SHALL NOT BE USED AS THE NEC REQUIRED EQUIPMENT GROUND CONDUCTOR. STRANDED COPPER CONDUCTORS WITH GREEN INSULATION, SIZED IN ACCORDANCE WITH THE NEC, SHALL BE FURNISHED AND INSTALLED WITH THE POWER CIRCUITS TO TRANSMISSION EQUIPMENT.
8. CONNECTIONS TO THE GROUND BUS SHALL NOT BE DOUBLED UP OR STACKED. BACK-TO-BACK CONNECTIONS ON OPPOSITE SIDES OF THE GROUND BUS ARE PERMITTED.
9. ALUMINUM CONDUCTOR OR COPPER CLAD STEEL CONDUCTOR SHALL NOT BE USED FOR GROUNDING CONNECTIONS.
10. USE OF 90° BENDS IN THE PROTECTION GROUNDING CONDUCTORS SHALL BE AVOIDED WHEN 45° BENDS CAN BE ADEQUATELY SUPPORTED. IN ALL CASES, BENDS SHALL BE MADE WITH A MINIMUM BEND RADIUS OF 8 INCHES.
11. EACH INTERIOR TRANSMISSION CABINET FRAME/PLINTH SHALL BE DIRECTLY CONNECTED TO THE MASTER GROUND BAR WITH #6 AWG STRANDED, GREEN INSULATED SUPPLEMENTAL EQUIPMENT GROUND WIRE UNLESS NOTED OTHERWISE IN THE DETAILS. EACH OUTDOOR CABINET FRAME/PLINTH SHALL BE DIRECTLY CONNECTED TO THE BURIED GROUND RING WITH #2 AWG SOLID TIN-PLATED COPPER WIRE UNLESS NOTED OTHERWISE IN THE DETAILS.
12. ALL EXTERIOR GROUND CONDUCTORS BETWEEN EQUIPMENT/GROUND BARS AND THE GROUND RING, SHALL BE #2 AWG SOLID TIN-PLATED COPPER UNLESS OTHERWISE INDICATED.
13. EXOTHERMIC WELDS SHALL BE USED FOR ALL GROUNDING CONNECTIONS BELOW GRADE. CONNECTIONS TO ABOVE GRADE UNITS SHALL BE MADE WITH EXOTHERMIC WELDS WHERE PRACTICAL OR WITH 2 HOLE MECHANICAL TYPE BRASS CONNECTORS WITH STAINLESS STEEL HARDWARE, INCLUDING SET SCREWS. HIGH PRESSURE CRIMP CONNECTORS MAY ONLY BE USED WITH WRITTEN PERMISSION FROM SAI COMMUNICATIONS MARKET REPRESENTATIVE.
14. EXOTHERMIC WELDS SHALL BE PERMITTED ON TOWERS ONLY WITH THE EXPRESS APPROVAL OF THE TOWER MANUFACTURER OR THE CONTRACTORS STRUCTURAL ENGINEER.
15. ALL WIRE TO WIRE GROUND CONNECTIONS TO THE INTERIOR GROUND RING SHALL BE FORMED USING HIGH PRESS CRIMPS OR SPLIT BOLT CONNECTORS WHERE INDICATED IN THE DETAILS.
16. ON ROOFTOP SITES WHERE EXOTHERMIC WELDS ARE A FIRE HAZARD COPPER COMPRESSION CAP CONNECTORS MAY BE USED FOR WIRE TO WIRE CONNECTORS. 2 HOLE MECHANICAL TYPE BRASS CONNECTORS WITH STAINLESS STEEL HARDWARE, INCLUDING SET SCREWS SHALL BE USED FOR CONNECTION TO ALL ROOFTOP TRANSMISSION EQUIPMENT AND STRUCTURAL STEEL.
17. COAX BRIDGE BONDING CONDUCTORS SHALL BE EXOTHERMICALLY BONDED OR BOLTED TO THE BRIDGE AND THE TOWER GROUND BAR USING TWO-HOLE MECHANICAL TYPE BRASS CONNECTORS AND STAINLESS STEEL HARDWARE.
18. APPROVED ANTIOXIDANT COATINGS (I.E., CONDUCTIVE GEL OR PASTE) SHALL BE USED ON ALL COMPRESSION AND BOLTED GROUND CONNECTIONS.
19. ALL EXTERIOR GROUND CONNECTIONS SHALL BE COATED WITH A CORROSION RESISTANT MATERIAL.
20. MISCELLANEOUS ELECTRICAL AND NON-ELECTRICAL METAL BOXES, FRAMES AND SUPPORTS SHALL BE BONDED TO THE GROUND RING, IN ACCORDANCE WITH THE NEC.
21. BOND ALL METALLIC OBJECTS WITHIN 6 FT OF THE BURIED GROUND RING WITH #2 AWG SOLID TIN-PLATED COPPER GROUND CONDUCTOR. DURING EXCAVATION FOR NEW GROUND CONDUCTORS, IF EXISTING GROUND CONDUCTORS ARE ENCOUNTERED, BOND EXISTING GROUND CONDUCTORS TO NEW CONDUCTORS.
22. GROUND CONDUCTORS USED IN THE FACILITY GROUND AND LIGHTNING PROTECTION SYSTEMS SHALL NOT BE ROUTED THROUGH METALLIC OBJECTS THAT FORM A RING AROUND THE CONDUCTOR, SUCH AS METALLIC CONDUITS, METAL SUPPORT CLIPS OR SLEEVES THROUGH WALLS OR FLOORS. WHEN IT IS REQUIRED TO BE HOUSED IN CONDUIT TO MEET CODE REQUIREMENTS OR LOCAL CONDITIONS, NON-METALLIC MATERIAL SUCH AS PVC PLASTIC CONDUIT SHALL BE USED. WHERE USE OF METAL CONDUIT IS UNAVOIDABLE (E.G., NON-METALLIC CONDUIT PROHIBITED BY LOCAL CODE) THE GROUND CONDUCTOR SHALL BE BONDED TO EACH END OF THE METAL CONDUIT WITH LISTED BONDING FITTINGS.

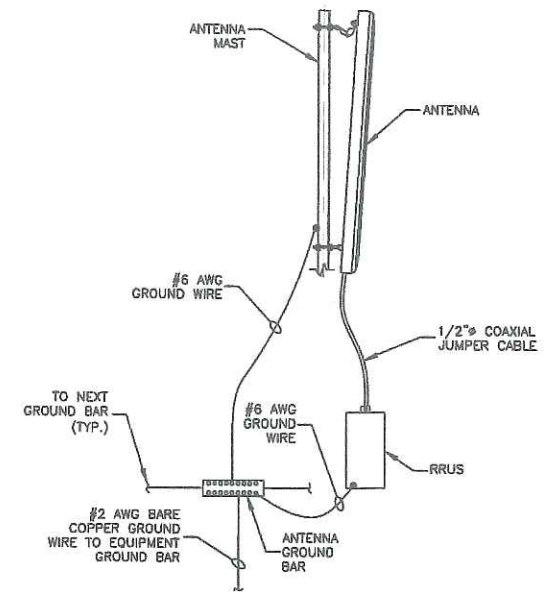


- NOTES:**
1. DOUBLING UP OR STACKING OF CONNECTIONS IS NOT PERMITTED.
 2. OXIDE INHIBITING COMPOUND TO BE USED AT ALL LOCATIONS.

TYPICAL GROUND BAR MECHANICAL CONNECTION DETAIL

SCALE: N.T.S.

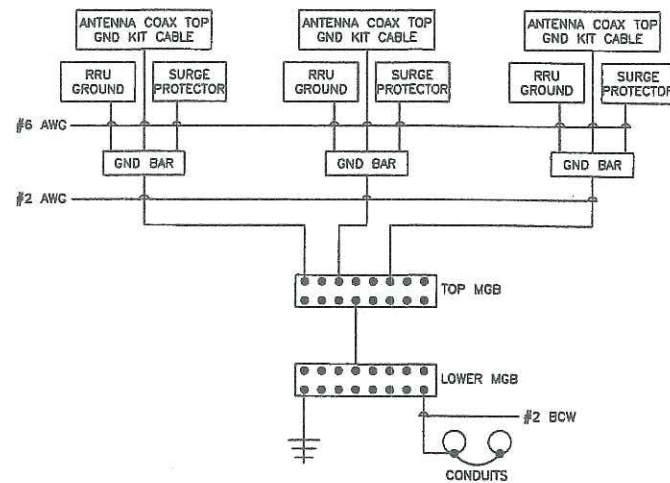
1



TYPICAL ANTENNA GROUNDING DETAIL

SCALE: N.T.S.

2



- NOTES:**
1. BOND ANTENNA GROUNDING KIT CABLE TO TOP CIGBE.
 2. BOND ANTENNA GROUNDING KIT CABLE TO BOTTOM CIGBE.

SCHEMATIC GROUNDING DIAGRAM

SCALE: N.T.S.

3

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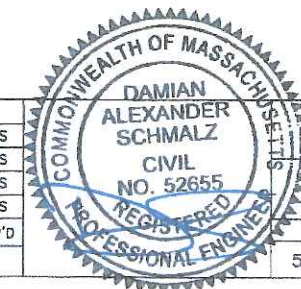
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1	10/07/16	ISSUED FOR CONSTRUCTION	MR	KB	DAS
0	08/18/16	ISSUED FOR REVIEW	MR	KB	DAS

SCALE: AS SHOWN DESIGNED BY: KB DRAWN BY: MR



AT&T MOBILITY
FRAMINGHAM, MA 01701
GROUNDING NOTES & DETAILS

DEWBERRY NO.	DRAWING NUMBER	REV
50019239/50083655	E01	3