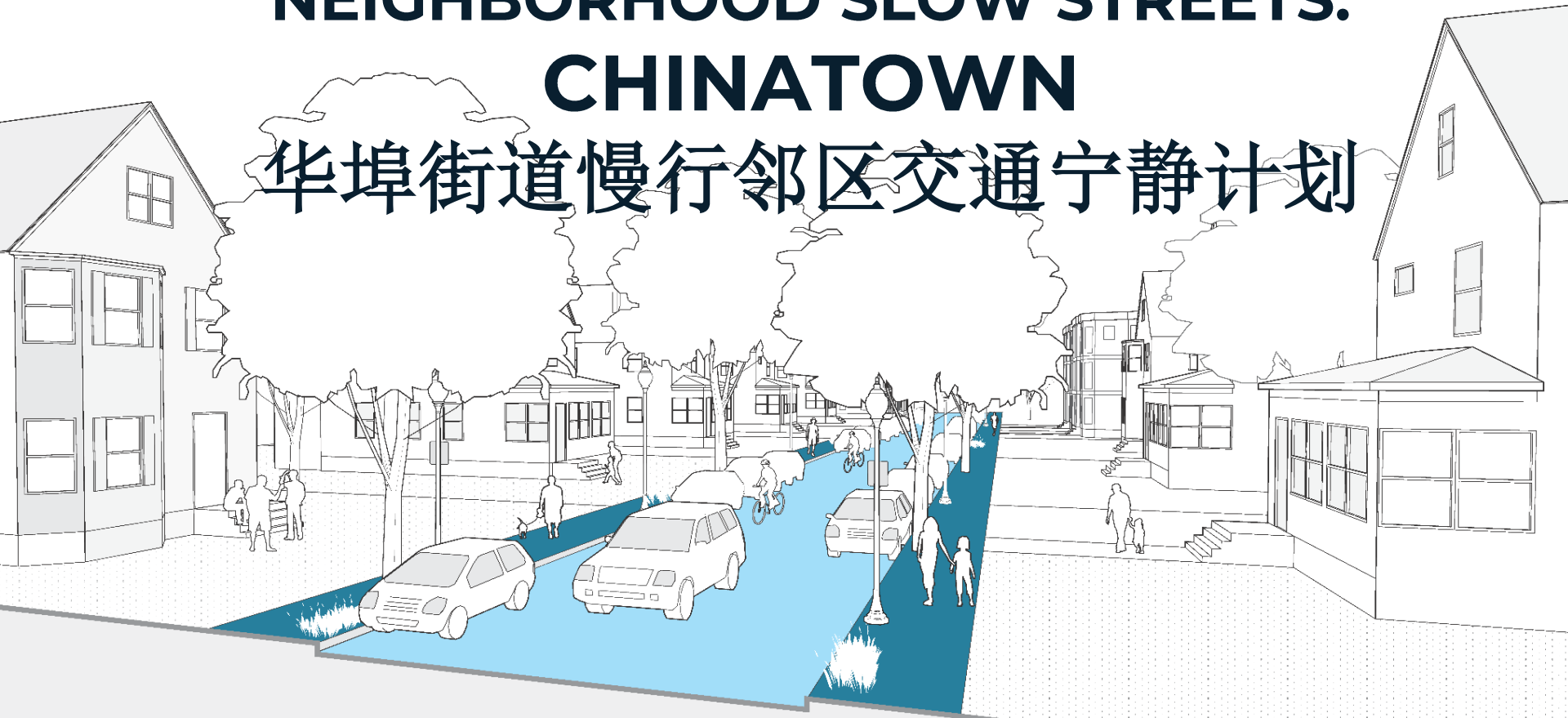


NEIGHBORHOOD SLOW STREETS: CHINATOWN

华埠街道慢行邻区交通宁静计划



MEETING GOALS

会议目的

- Introduce more residents to the Neighborhood Slow Streets project
介绍华埠街道慢行邻区交通宁静计划给更多居民知道。
- Share our design plans for final feedback
分享我们的最后设计方案和听取意见

VISION ZERO BOSTON

波士顿“零交通事故愿景”

- Commitment to eliminate all fatal and serious injuries by 2030
承诺到2030年要消除所有致命和严重的伤害
- Designing for the most vulnerable benefits everyone
为最容易受伤害的人去设计
- “Early Action” policy in Go Boston 2030
波士顿2030年的“早期行动”政策



SLOWER SPEEDS, SAFER STREETS

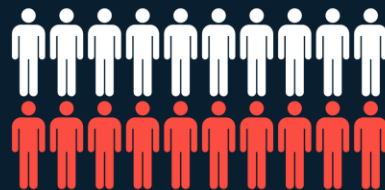
降低速度，更安全的街道



17% likelihood of fatal or severe injury



30% likelihood of fatal or severe injury



47% likelihood of fatal or severe injury

NEIGHBORHOOD SLOW STREETS

街道慢行邻区

A zone-based approach to traffic-calming requests

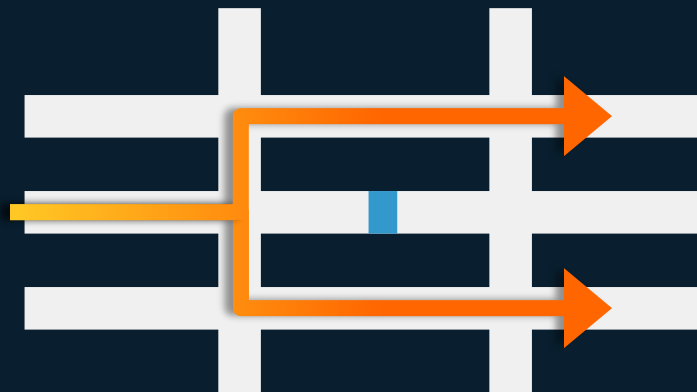
用区域的方法进行交通宁静的请求

- A bounded area of local streets
当地街道的限定区域
- Speed limit reduced to 20 MPH
速度限制降至20英里/小时
- Self-enforced lower speeds through targeted traffic-calming techniques
通过有针对性的交通镇定技术自我强制执行较低的速度

WHY ZONE-BASED?

为什么选择区域?

- Avoid the “**transfer effect**” 避免“转移效应”
- Drivers seek alternate routes 司机寻求替代路线
- Traffic and/or speeds may increase on parallel routes without traffic calming 在没有交通慢行区的情况下，并行路线上的交通或速度可能会增加



PROGRAM GOALS

计划目标

- Reduce vehicle speeds
降低车速
- Fewer crashes resulting in injury or property damage
减少碰撞而引致的人身伤害或财产损失
- Improved perception of safety
提高安全性的认识
- Quality of life benefits
提高生活质量

YOUR PARTICIPATION INFORMED THIS PLAN

你想参与此计划可以参与以下：

OCTOBER 3, 2017
2017年10月3日

Community Walk
社区步行

JULY 26, 2018
2018年7月26日

Public Meeting (China Trade Building)
公共会议（中华贸易大楼）

AUGUST 1, 2018
2018年8月1日

Chinatown Residents Association
华埠居民会

AUGUST 20, 2018
2018年8月20日

Chinatown Neighborhood Council
华埠区议会

SEPTEMBER 5, 2018
2018年9月5日

Accepted feedback online, by phone or mail
接受反馈意见，网上、电话或邮件

MAY 7, 2019
2019年5月7日

Public Meeting (Quincy BCYF)
公共会议（昆士小学）

MAY 20, 2019
2019年5月20日

Chinatown Neighborhood Council
华埠区议会

JUNE 5, 2019
2019年6月5日

Chinatown Residents Association
华埠居民会

TOP CONCERNS

最关注的问题

- Poor visibility at crosswalks
人行横道的能见度差
 - People park right next to crosswalks
人们停在人行横道旁边
- People driving do not yield to people in crosswalks
开车的人不会让行人
- Considerations for Emergency vehicles
紧急车辆的考虑因素

Chinatown
城國中
Resident
Permit
Parking

CHINATOWN DESIGN PLAN

唐人街设计方案



ZONE-WIDE: SPEED LIMIT REDUCTION

区域：速度限制

As people enter the zone

如果人们进来这个区域:

- 20 MPH speed limit signs
20英里/小时的限速标志
- Supplemented with 20
MPH pavement marking
补充20英里/小时路面标记



DAYLIGHTING

显眼

Improves visibility for drivers,
pedestrians

提高司机，行人的能见度

Restricts parking 20' from
intersection, per City rule

规定路口有限速20'的标志

Generally on approach to
crosswalk

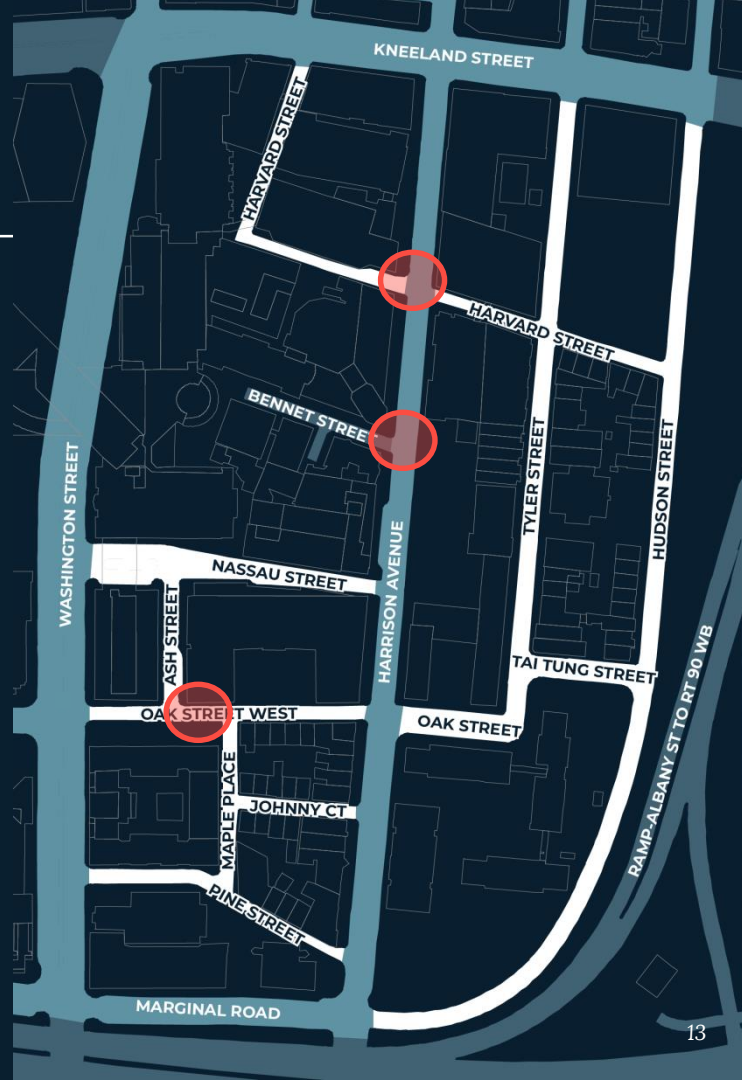
行人要用人行横道



DESIGN PLAN

设计方案

- *Daylighting* 显眼
- Raised crosswalks 人行横道



RAISED CROSSWALK

人行横道

- Crosswalk is level or near-level with sidewalk
人行横道是水平或接近水平的人行道
- Slows drivers at intersection
交叉路口让司机减速
- Improves visibility of people crossing the street
提高过马路的人的能见度
- Easier for people with wheelchairs, walkers, strollers, carts
轮椅，助行器，婴儿车，推车等行人更容易



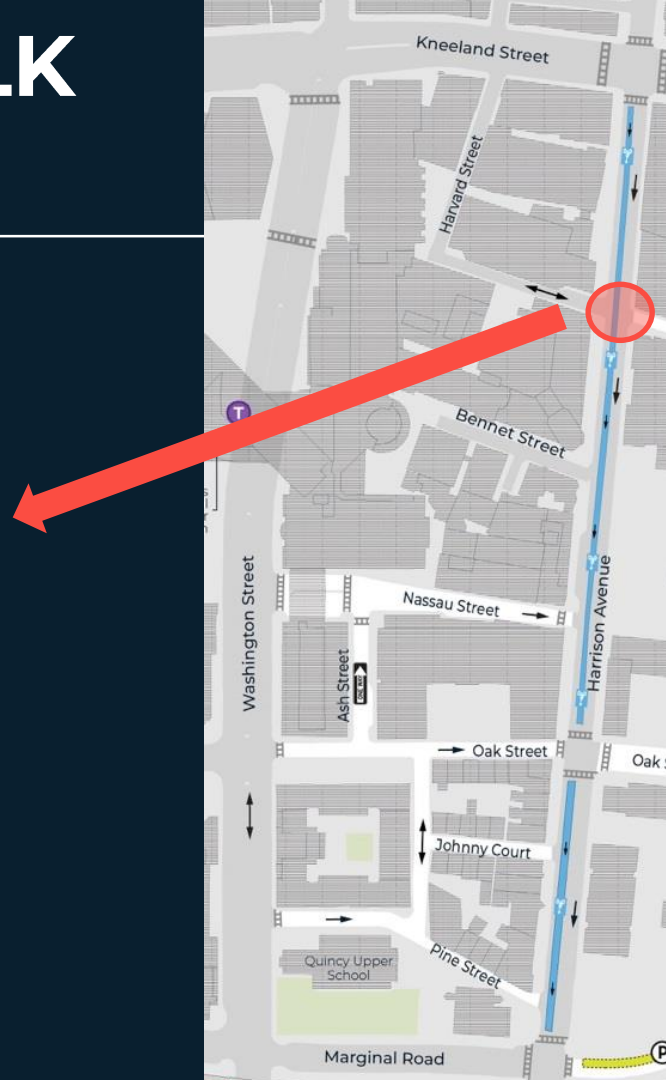
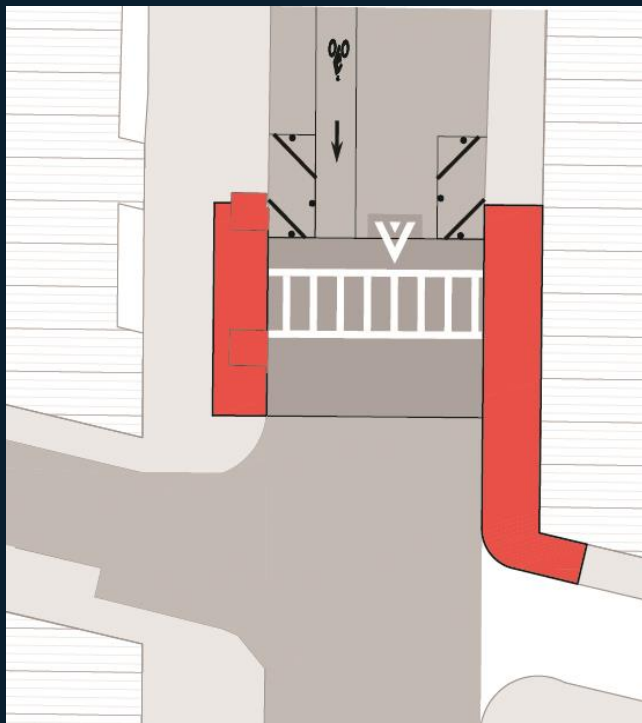
RAISED CROSSWALK

人行横道



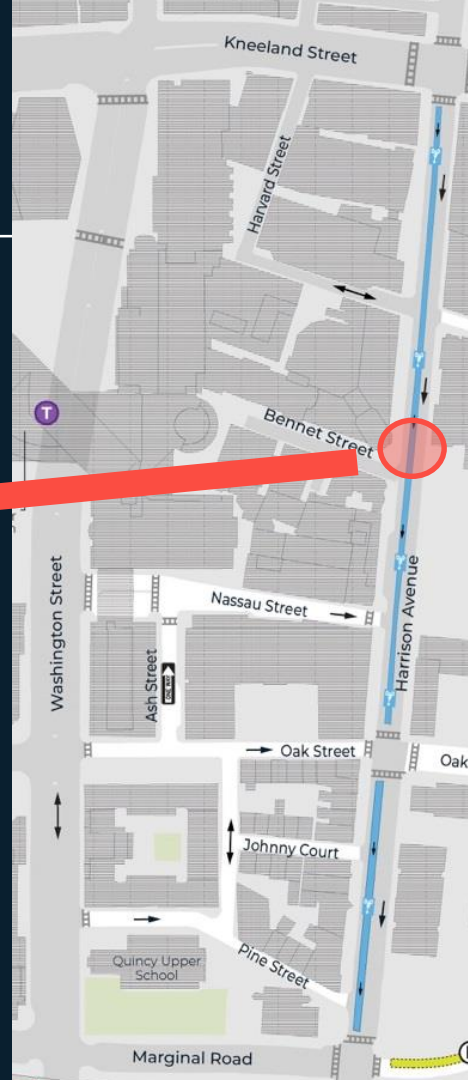
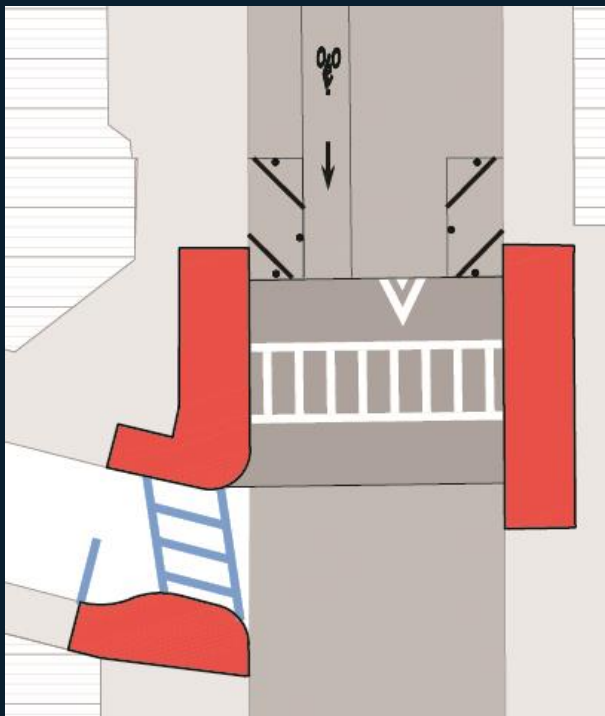
RAISED CROSSWALK

人行橫道



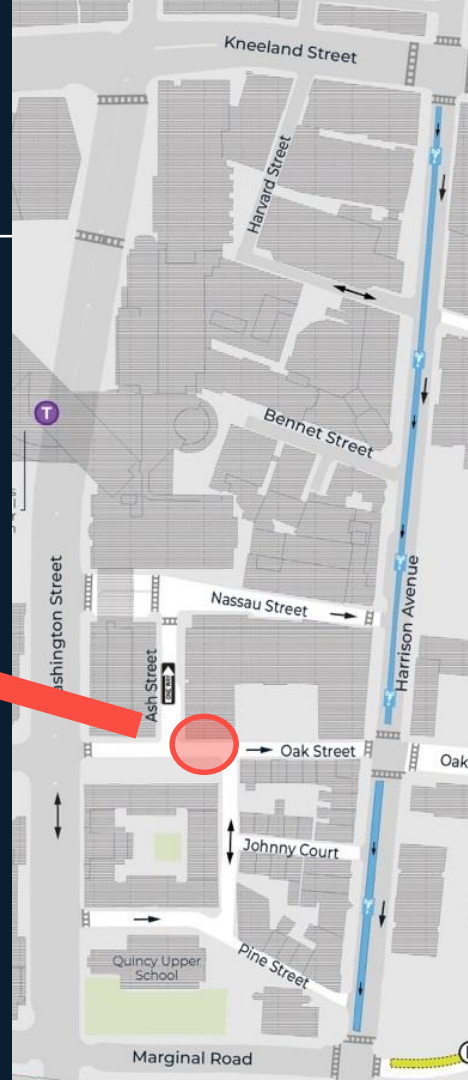
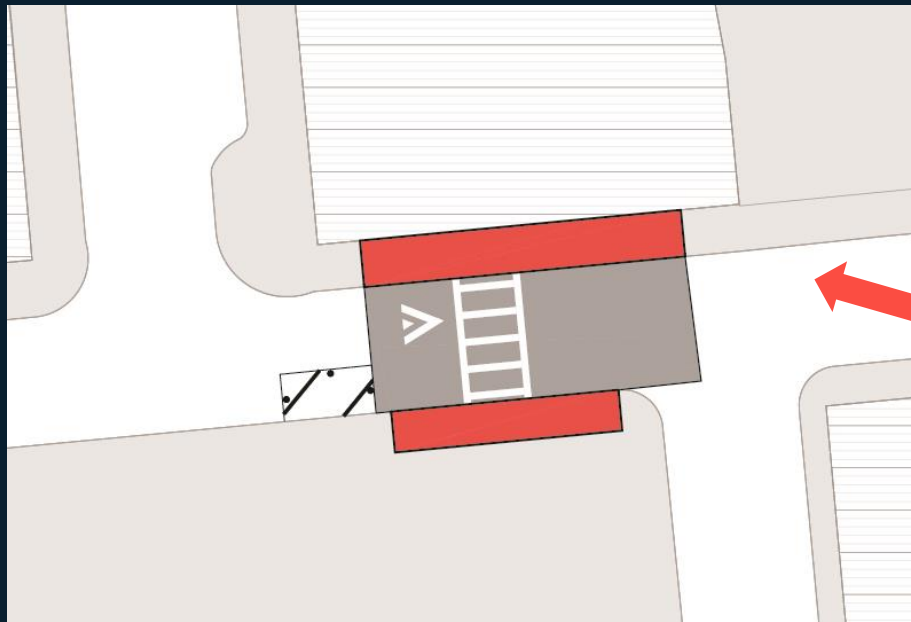
RAISED CROSSWALK

人行橫道



RAISED CROSSWALK

人行橫道



DESIGN PLAN

设计方案

- *Daylighting* 显眼
- *Raised crosswalks* 人行横道
- *Speed humps* on Hudson St 乞臣街的减速带



SPEED HUMPS ARE NOT SPEED BUMPS!

減速帶！



YES! Gradual taper up and down, 12 to 14 feet long 正确！
上下逐渐变细，12到14英尺长



NO! Abrupt, hard bump, 3 to 4 feet long 不是这样的！ 突然的，硬凸块，3到4英尺长

SPEED HUMPS

减速带

- Located on property lines (where feasible) 位于物业线 (如果可行)
- Not located at driveways or intersections 不在车道或十字路口
- Cannot be used on hills or too close to a curve 不能在山上使用或太靠近曲线



HUDSON ST SPEED HUMPS

乞臣街的减速带

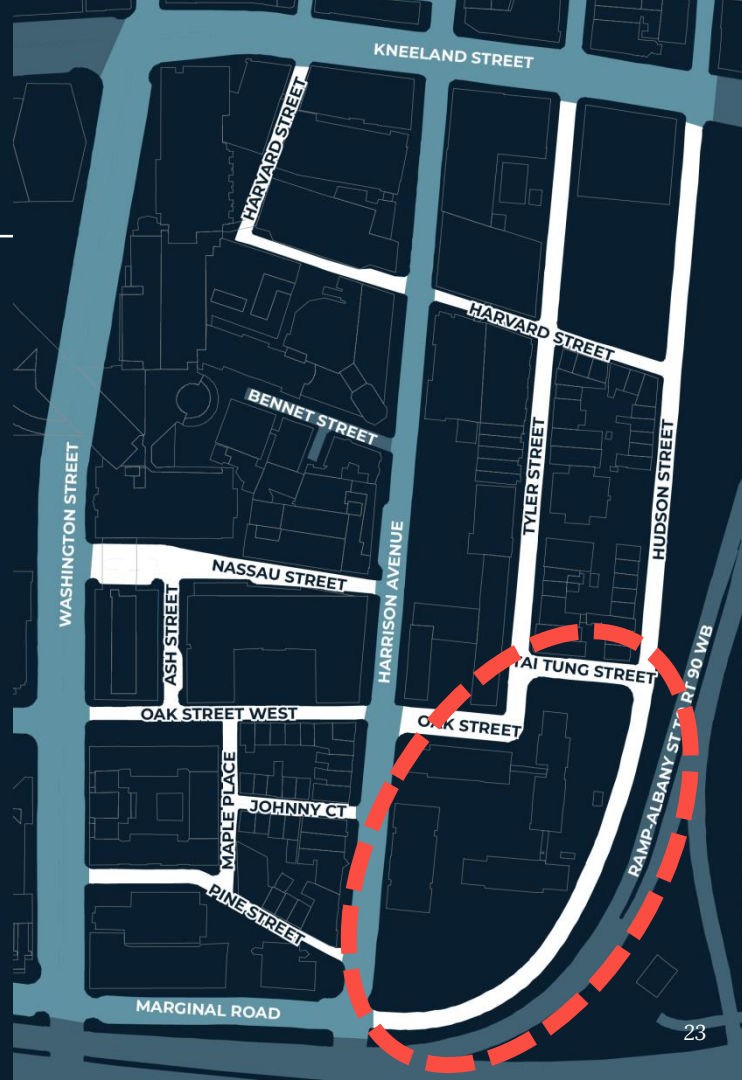
- Spaced approx. 150'-250'
间隔大约150'-250'
- No impact on parking对
停车没有影响
- No impact on drainage
对排水没有影响



DESIGN PLAN

设计方案

- *Daylighting* 显眼
- *Raised crosswalks* 人行横道
- *Speed humps on Hudson St*
乞臣街的减速带
- *Chicane on Hudson St*
乞臣街的急弯



HUDSON ST CHICANE

乞臣街的急弯

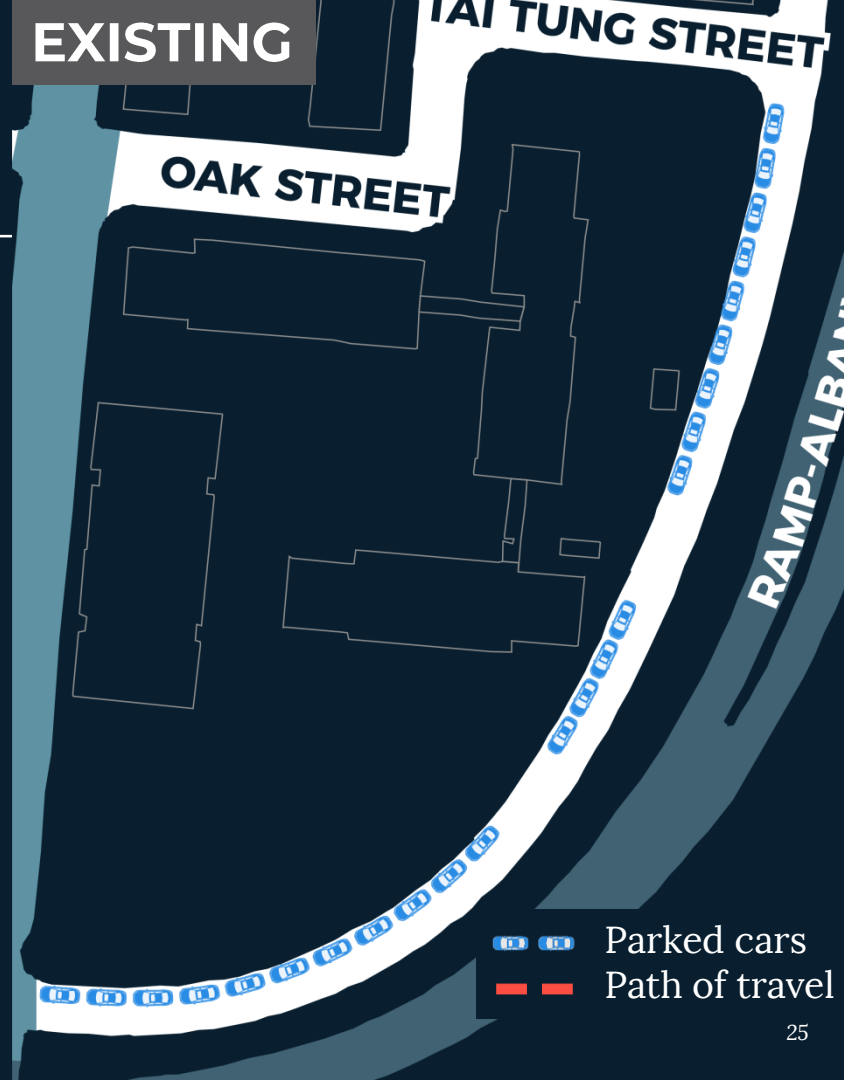
- Hudson St was the only street we found drivers going more than 20 mph 乞臣街是我们发现司机行驶速度超过20英里/小时的唯一街道
- Parking lane switches sides to create a curve 停车道切换侧面以创建曲线
- Curve slows drivers 曲线减慢了司机的速度



HUDSON ST CHICANE

乞臣街的急弯

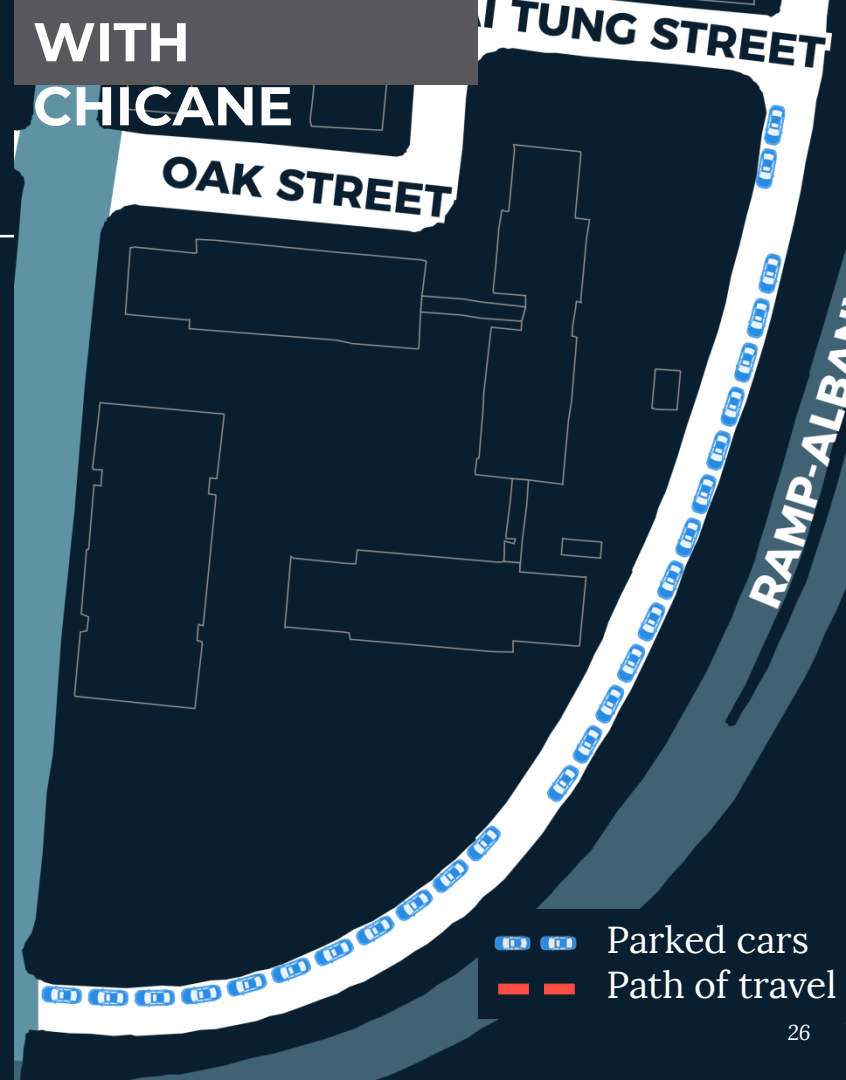
- Parking lane switches sides to create a curve
停车道切换侧面以创建曲线
- Curve slows drivers
曲线减慢了司机的速度



HUDSON ST CHICANE

乞臣街的急弯

- Parking lane switches sides to create a curve 停车道切换侧面以创建曲线

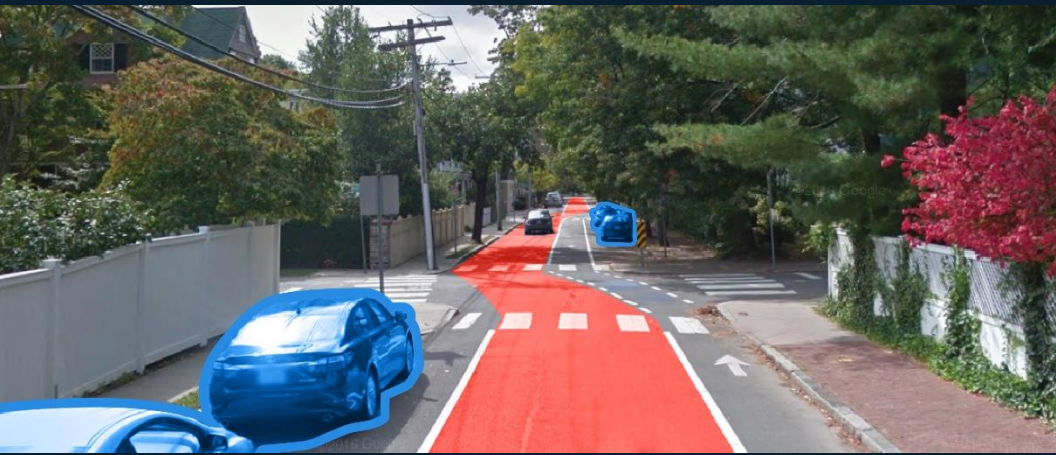


Sparks St, Cambridge, MA Image source: Google maps

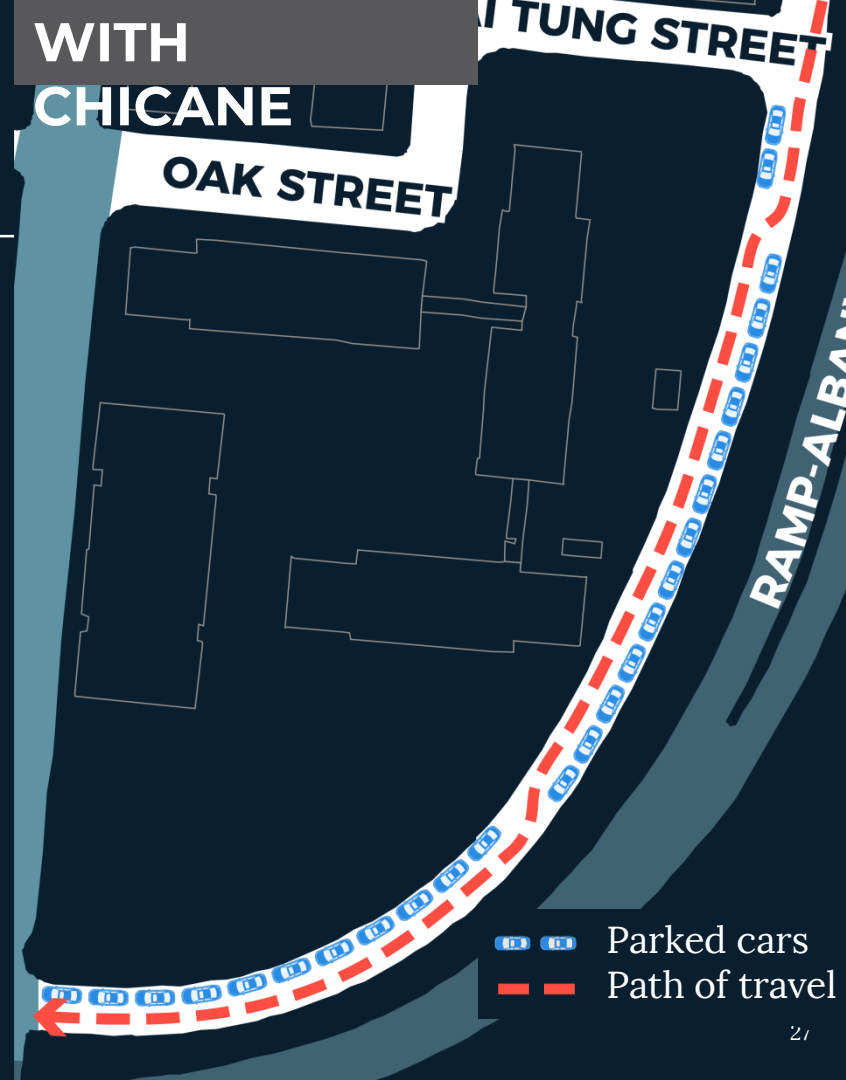
HUDSON ST CHICANE

乞臣街的急弯

- Parking lane switches sides to create a curve 停车道切换侧面以创建曲线



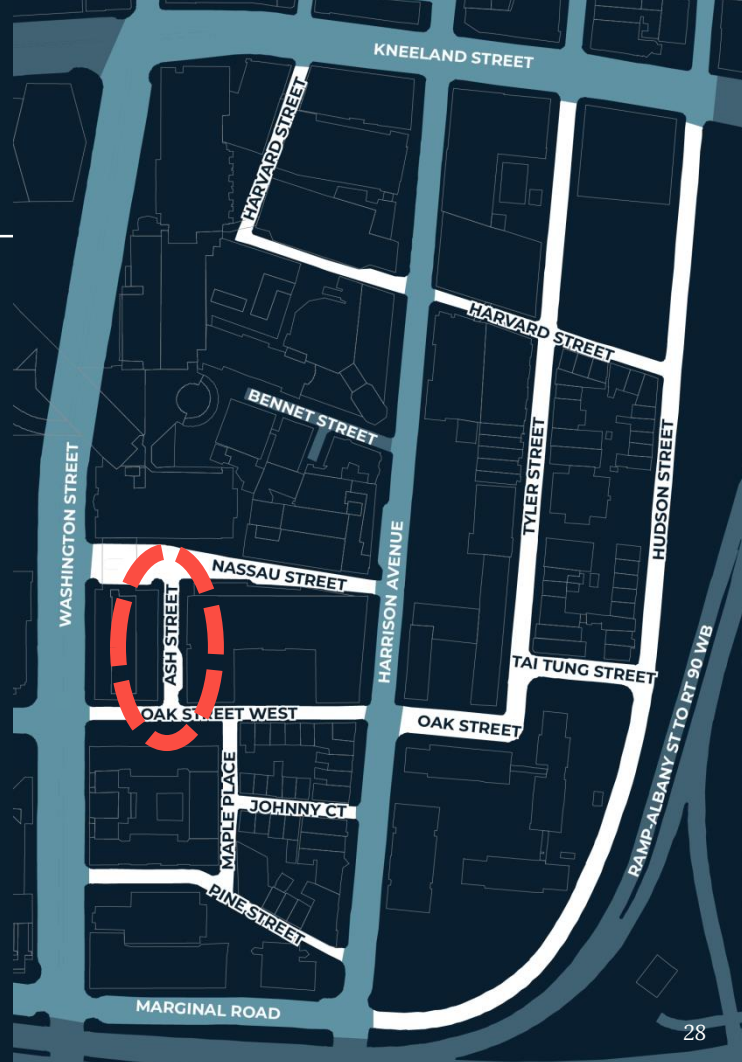
Sparks St, Cambridge, MA Image source: Google maps



DESIGN PLAN

设计方案

- Daylighting 显眼
- Raised crosswalks 人行横道
- Speed humps on Hudson St 乞臣街的减速带
- Chicane on Hudson St 乞臣街的急弯
- One-way change to Ash St 雅树街改为单向道



ONE WAY CHANGE TO ASH ST

雅树街改为单向道

- Propose making Ash St one-way northbound
建议将雅树街改为单向道



PARKING IMPACTS (+/-)

泊车影响 (+/-)

-1 one-hour parking, 8 am – 4 pm

-1 早上8点至下午4点, 1小时泊车

-6 Resident parking -6 居民泊车

+5 Resident parking +5 居民泊车

-2 Metered 8 am – 6 pm / 2-hour 6 pm – 10 pm, Mon – Sat

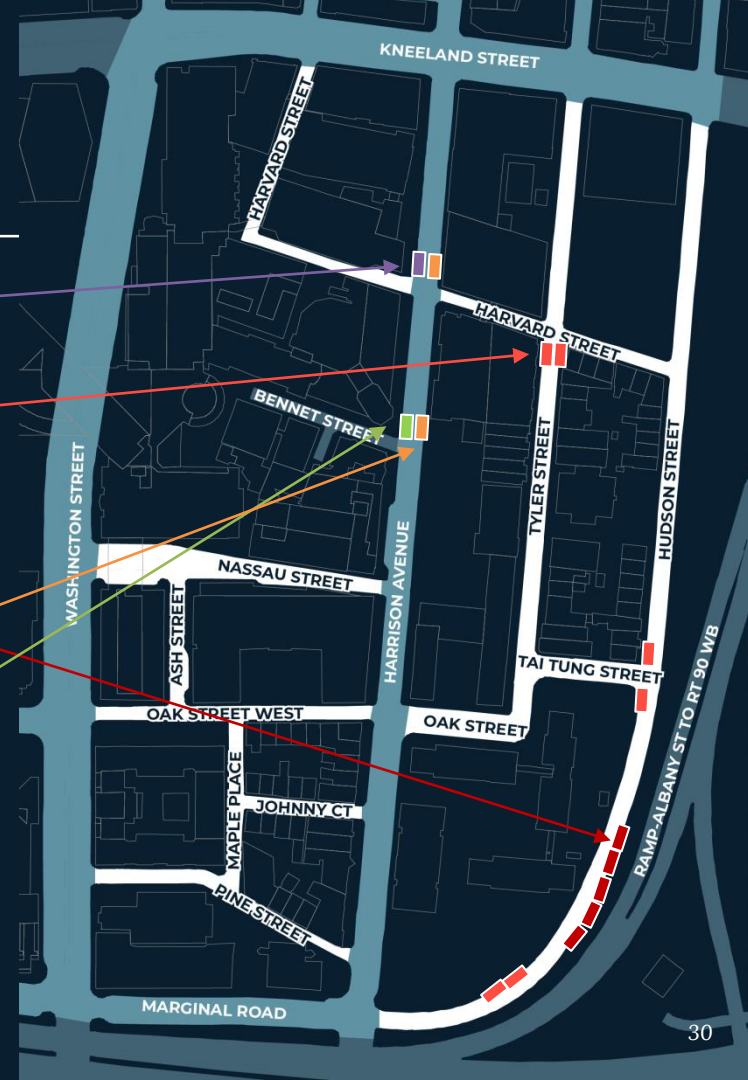
-2 咪表: 早上8点 - 下午6点 / 2小时, 下午6点 - 晚上10点,
星期一至星期六

-1 Tufts patient, 6 am – 6 pm, Mon – Fri

-1 塔芙医院病人, 星期一, 早上6点至下午6点

-5 Total Parking Spaces

-5 总共车位



OUR NEXT STEPS

我们的下一步

MAY - JUNE 2019

2019年5-6月

- Public meeting # 2 to share final plans 第二次的公共会议分享最后方案
- Additional meetings with neighborhood groups 与社区团体的其他会议
- Refine design, as needed, based on your and your neighbors' feedback 根据您和您邻居的反馈，根据需要优化设计

SUMMER 2019

2019年夏天

- Seek approval from Public Improvements Commission 寻求公共改进委员会的批准
- Final design review by BTM and PWD 通过波士顿交通部门和PWD最终设计审查
- Bid construction contract 投标施工合同
- Schedule construction with Public Works (*Construction is fully-funded for Fiscal Year 2020, covering July 2019 through June 2020*) 计划建设与公共工程 (2020 财政年, 时间是2019年7月至2020年6月)

THANK YOU 谢谢!

boston.gov/slow-streets/chinatown
visionzero@boston.gov